

Executive Summary

Study Initiation

This study was initiated by the City of Trussville through the Advanced Planning, Programming, and Logical Engineering (APPLE) program developed by the Regional Planning Commission of Greater Birmingham (RPCGB). The City requested professional planning assistance in evaluating the feasibility of a trail that would follow Pinchgut Creek through the City of Trussville. The alignment of Pinchgut Creek provides an ideal trail location in the City of Trussville since it connects the commercial areas and residential developments along US-11. A map showing the study area is shown in **Figure 1**.

Purpose for the Study

This study was undertaken to assess the feasibility of constructing a trail that would follow Pinchgut Creek through the City of Trussville from the Homestead Village shopping center to the existing greenway at Civitan Park. The purpose of this study is to identify improvements, and through the development of this report, provide stakeholders with information for their use in decision-making. This document summarizes:

- existing conditions,
- the process used to identify potential improvements,
- an evaluation of potential positive and negative impacts to the area and adjacent properties that may be associated with each potential improvement,
- funding options
- stakeholder input.

The primary goal of constructing a trail following Pinchgut Creek is to provide continuous active transportation connectivity between the existing commercial and residential land uses within the study area.

Proposed Improvements

The study area was divided into four segments based on existing projects, surrounding infrastructure, logical termini, and engineering judgment. Conceptual diagrams of improvements can be found in Section 4 and **Appendix A** of this report. The following proposed improvements are recommended within the study area:

Section A – Veterans Memorial at Civitan Park to City Hall Drive

- Construct a 10-foot wide path from Veterans Memorial at Civitan Park to the intersection of City Hall Drive and Railroad Avenue. Use either concrete or a boardwalk surface, depending on the location along the segment (See **Appendix A: Section A Concept Plan**).
- Install a crosswalk on the southern leg of the City Hall Drive at Railroad Avenue intersection.

Section B – City Hall Drive to the Trussville Entertainment District

- Remove and replace the existing sidewalk along the north side of Railroad Avenue to a width of 10 feet. Install new sidewalk to connect the existing sidewalks on Railroad Avenue and Chalkville Road.
- Install a rectangular rapid flashing beacon (RRFB) and marked crosswalk on the northern leg of the Chalkville Road intersection with Railroad Avenue.
- Construct a boardwalk structure from the former Golden Rule BBQ site to the southwest corner of the Trussville Entertainment District, just north of Pinchgut Creek. Install a 10-foot wide concrete path on either end of the boardwalk to connect it to the Chalkville Road crosswalk and proposed entertainment district trailhead in Section C.

Section C – Trussville Entertainment District to Winn Dixie Shopping Center

- Construct a 10-foot wide concrete path along the north side of Pinchgut Creek from the Trussville Entertainment District's southwest corner to Winn Dixie Shopping Center's southwest corner. For the subsection of proposed trail along the Pinchgut Creek easement, the path should be constructed at the base of the existing spoil bank where terrain flattens out. Where required, construct a structure for the trail to span the drainage channel just east of Watterson Parkway.
- Construct a trailhead at each end of Section C. One trailhead is proposed adjacent to the Trussville Entertainment District, and a second trailhead is proposed at the southwest corner of the Winn Dixie Shopping Center's parking lot.

Section D – Winn Dixie Shopping Center to Homestead Village Shopping Center

- Construct a 10-foot wide concrete path along the north side of Pinchgut Creek from the Section C path to the existing sidewalk within the Homestead Village Shopping Center.
- Due to the environmentally sensitive nature of the parcels within Section D, asphalt or natural surface trail may be necessary instead of concrete. Reevaluate during the design phase and adjust plans as necessary.
- If the grade differential between the Homestead Village Shopping Center parking lot and the low-lying areas of Section D render ADA-compliance infeasible, install adequate signage at each end of proposed trail for Section D to warn trail users.

Conceptual diagrams of proposed improvements can be found in **Appendix A**.

Stakeholder Involvement

A kickoff meeting was held on June 29, 2022, at Trussville City Hall to discuss the project background, identify study priorities, and determine expectations for the study. Representatives from the City of Trussville, Regional Planning Commission of Greater Birmingham (RPCGB), and Sain Associates were present.

A second meeting was held on November 16, 2022, at the Trussville City Hall Annex to discuss findings from existing conditions, preliminary trail alignments, deliverables, and next steps for the study. Representatives from the City of Trussville, Regional Planning Commission of Greater Birmingham (RPCGB), PLOT Studio, and Sain Associates were present.

A third meeting was held on April 4, 2023, at the Trussville City Hall Annex to present the results of the study to representatives from several local businesses in the study area. Feedback from the local business representatives was positive towards the study results. Representatives from the City of Trussville, Regional Planning Commission of Greater Birmingham (RPCGB), PLOT Studio, and Sain Associates were present.

Next Steps

The City has stated a preference for using local funds to construct the project. If locally funded, the timing, scheduling, and implementation of the installation would be at their discretion.

If instead the City chooses to move forward with implementing any portion of the proposed improvements with federal funding, the next step would be to request inclusion of a project in RPCGB's Transportation Improvement Plan (TIP) for Fiscal Years 2024 – 2027. The preparation of this feasibility study can be used in the application for funds from the RPCGB for future improvements.

Once Federal funds are in place for the project, an environmental document will need to be prepared. The environmental document must include technical studies and public involvement outreach necessary to comply with procedures of the National Environmental Policy Act (NEPA). Once the environmental study has been completed, the design would be undertaken, and construction would follow. Right-of-way acquisition and utility relocation would be conducted prior to construction.

Table of Contents

1	Introduction.....	1
1.1	Purpose of the Study	1
1.2	Study Approach	1
2	Existing Conditions	3
2.1	Description of the Study Area.....	3
2.2	Bicycle and Pedestrian Accommodations	4
2.3	Roadway Geometrics	6
2.3	Existing Parks and Recreational Areas	7
2.4	Previous Documents and Adjacent Projects	9
2.5	Field Observations	10
3	Environmental Features.....	21
3.1	Historic Assets	21
3.2	Section 4(f) Properties	22
3.3	Threatened and Endangered Species	22
3.4	Streams and Wetlands	23
3.5	Prime and Unique Farmlands	24
3.6	Hazardous Materials Properties	25
3.7	Environmental Justice	25
3.8	Utilities	26
4	Improvement Alternatives	28
4.1	Section A: Veterans Memorial at Civitan Park to City Hall Drive	29
4.2	Section B: Trussville Entertainment District to City Hall Drive	29
4.3	Section C: Trussville Entertainment District to Winn Dixie Shopping Center	33
4.4	Section D: Winn Dixie to Homestead Village Shopping Center.....	35
4.5	Opinions of Probable Cost.....	36

5	Accessibility	37
6	Funding Sources.....	37
6.1	Federal Funding.....	38
6.2	State Funding.....	38
6.3	Local Funding	38
7	Stakeholder Involvement	39
8	Next Steps.....	39

Appendices

- Appendix A – Proposed Improvement Concepts
- Appendix B – Study Area Right-of-Way Map
- Appendix C – USFWS Threatened and Endangered Species
- Appendix D – NRCS Web Soil Survey
- Appendix E – EPA Environmental Justice Screening Tool Summary
- Appendix F – Opinions of Probable Cost

List of Figures

Figure 1: Study Area Location Map.....	2
Figure 2: Key Locations within the Study Area.....	3
Figure 3: Existing Parks, Recreational Facilities, and Trail Systems	8
Figure 4: Railroad Avenue Area	11
Figure 5: USACOE Pinchgut Creek Right-of-Way Typical Section	26
Figure 6: Conceptual Map of Proposed Improvements	28
Figure 7: Section A Conceptual Diagram of Proposed Improvements	29
Figure 8: Section B Conceptual Diagram of Proposed Improvements	30
Figure 9: Railroad Avenue Section Perspective	31
Figure 10: Typical Trail Furniture and Lighting (All Sections)	31
Figure 11: Section Perspectives for Proposed Trail at Trussville Entertainment District	32
Figure 12: Section C Conceptual Diagram of Proposed Improvements	33
Figure 13: Section Perspective along Pinchgut Creek between Winn-Dixie and Watterson Parkway	34
Figure 14: Section Perspective of Proposed Trailhead at Winn-Dixie Parking Lot.....	34
Figure 15: Section D Conceptual Diagram of Proposed Improvements	35

List of Tables

Table 1: USFWS IPaC Results	23
Table 2: Environmental Justice Screen Tool Summary	25
Table 3: Opinions of Probable Cost Proposed Improvements	36
Table 4: Funding Options	37

List of Photos

Photo 1: Existing Trussville Greenway at Civitan Park.....	4
Photo 2: Bicycle Racks at Veterans Memorial.....	4
Photo 3: Trussville Entertainment District Entrance along Beech Street.....	5
Photo 4: Homestead Village Shopping Center Existing Sidewalk.....	5
Photo 5: Concrete Path near Veterans Memorial at Civitan Park	7
Photo 6: View of US-11 bridge over the Cahaba River (Potential Trail Location)	10
Photo 7: Pinchgut Creek under Chalkville Road.....	12
Photo 8: Chalkville Road between US-11 and Pinchgut Creek	12
Photo 9: Birds-Eye View of the Trussville Entertainment District and Surrounding Area	13
Photo 10: Grassed Area between Entertainment District and Pinchgut Creek	13
Photo 11: Drainage Pipe under Trussville Entertainment District Stage	14
Photo 12: Headwall near Entertainment District Loading Docks	14
Photo 13: Birds-eye View of the Area between the Entertainment District and Watterson Parkway	15
Photo 14: Drainage Channel Terminus at Pinchgut Creek	15
Photo 15: Typical View of the Easement along Pinchgut Creek in the Study Area, Looking Southwest	16
Photo 16: Birds-eye View of Pinchgut Creek Easement	16
Photo 17: Easement with reference to the Winn Dixie Shopping Center	17
Photo 18: Headwall and Drainage Channel at Southwest Corner of Winn Dixie Parking Lot	17
Photo 19: View of the Drainage Channel Separating Sections C and D	18
Photo 20: Birds-eye View of the Homestead Village Shopping Center	19
Photo 21: Utilities along Northwestern Edge of Hobby Lobby Parking Lot.....	19
Photo 22: Utilities along the Southwestern Edge of Hobby Lobby Parking Lot	19
Photo 23: Existing Sidewalk at Homestead Village Shopping Center	20
Photo 24: Happy Hollow District Bridge at Civitan Park.....	22
Photo 25: View of Pinchgut Creek	24
Photo 26: Overhead View of the USACOE-Jefferson County Easement	27

1 Introduction

This study was initiated by the City of Trussville through the Advanced Planning, Programming, and Logical Engineering (APPLE) program developed by the Regional Planning Commission of Greater Birmingham (RPCGB). The City requested professional planning assistance in evaluating the feasibility of constructing a trail that would follow Pinchgut Creek through the City of Trussville. A continuous trail in this location would serve to connect the commercial areas and residential developments along US-11. A map showing the study area is shown in **Figure 1**.

1.1 Purpose of the Study

This study was undertaken to assess the feasibility of constructing a trail that would follow Pinchgut Creek through the City of Trussville from the Homestead Village shopping center to Civitan Park. The purpose of the study is to identify improvements, and through the development of this report, provide stakeholders with information for their use in decision-making. This document summarizes:

- existing conditions,
- the process used to identify potential improvements,
- an evaluation of potential positive and negative impacts to the area,
- funding options, and
- stakeholder input.

The primary goal of constructing a trail following Pinchgut Creek is to provide continuous active transportation connectivity between the existing commercial and residential land uses within the study area.

1.2 Study Approach

The study consisted of a two-stage process. The first stage included an evaluation of the existing conditions, a review of past studies, and definition of constraints within the study area. The second stage included developing conceptual plan alternatives. A detailed, quantitative traffic analysis was deemed unwarranted due to the land use of the study area and the study's focus on pedestrians and cyclists. Where necessary for determining appropriate bicycle and pedestrian accommodations, traffic data was obtained from the ALDOT Traffic Data website. A base map was prepared using aerial images and available GIS data. This field review consisted of assessing existing bicycle and pedestrian accommodations within the study area and establishing potential alignments for new location trail along Pinchgut Creek. Stage two included an evaluation of potential right-of-way (ROW) impacts and Americans with Disabilities Act (ADA) considerations. Conceptual plan renderings, section perspectives, and an evaluation matrix for each segment of the proposed trail were developed.



Figure 1: Study Area Location Map

Pinchgut Creek Trail APPLE Study
Trussville, Alabama

2 Existing Conditions

2.1 Description of the Study Area

The study area is located south of US-11 in Trussville, Alabama. The proposed trail would be constructed adjacent to Pinchgut Creek, starting at the Homestead Village shopping center and continue to the existing Trussville Springs trail system, where Pinchgut Creek meets the Cahaba River. The Homestead Village shopping center contains restaurants, a grocery store, and retail stores. At the eastern end of the study area is Trussville City Hall, police department, water treatment facility, Veterans Memorial at Civitan Park, and the Trussville Springs subdivision. The Veterans Memorial at Civitan Park is located just north of US-11, with a connection to an existing greenway following the Cahaba River through Civitan Park. Trussville Springs is a residential development between the Cahaba River and US-11 on the eastern end of the study area. Trussville Springs has an existing trail, but it lacks connection to the greenway at Civitan Park or the municipal campus.

Figure 2 shows several key locations within the study area. A map of the existing parcel data and utilities can be found in **Appendix B**.



Figure 2: Key Locations within the Study Area

2.2 Bicycle and Pedestrian Accommodations

Several bicycle and pedestrian accommodations exist within the study area. The downtown street grid is well-outfitted with sidewalks, providing excellent connectivity to local businesses. A concrete-paved greenway runs along the Cahaba River within Civitan Park, connecting the Veterans Memorial to Masonic Park, Cahaba Bend Elementary, the Trussville Mall, Cahaba Dog Park, Civitan Disc Golf Course, and the Trussville Sports Complex. **Photo 1** shows the existing greenway at Civitan Park.



Photo 1: Existing Trussville Greenway at Civitan Park

Veterans Memorial at Civitan Park is adjacent to US-11 on the southern end of the existing greenway. Bicycle racks have been installed at the Veterans Memorial, as shown in **Photo 2**.



Photo 2: Bicycle Racks at Veterans Memorial

The Trussville Entertainment District is situated in the center of downtown in the southwest quadrant of Main Street and Chalkville Road. The area is well-outfitted with sidewalks and provides dining options, shopping, and an event venue for residents to enjoy. **Photo 3** shows the entrance to the entertainment district.



Photo 3: Trussville Entertainment District Entrance along Beech Street

On the west end of the study area, the Homestead Village Shopping Center provides dining and shopping options along US-11. Existing sidewalks provide access to the residential neighborhood across US-11 via a signalized intersection with pedestrian push-buttons and signal heads. **Photo 4** shows the sidewalk along the main access to the Homestead Village Shopping Center.



Photo 4: Homestead Village Shopping Center Existing Sidewalk

2.3 Roadway Geometrics

Several key streets within the study area would need to be crossed or otherwise incorporated into a proposed trail.

US-11 (State Route 7) is a five-lane, urban minor arterial roadway with a posted speed limit of 35 MPH within the study area. Pavement width ranges from 52 to 64 feet throughout the study area, including a two-way left turn lane which transitions to left turn lanes at intersections. According to the ALDOT Traffic Data website, the AADT (Average Annual Daily Traffic) volume in 2021 along US-11 within the study area ranged from 26,192 on the western side of the study area to 17,187 on the eastern side of the study area.

The **Homestead Village Access** is a two-lane, local roadway intersecting US-11 and Tutwiler Drive on the western end of the study area. This intersection is signalized and possesses pedestrian accommodations in the form of crosswalks, sidewalks, and pedestrians signal heads. The Homestead Village Access has a posted speed limit of 15 MPH.

Kenimer Avenue is a two-lane local roadway within the study area, currently providing unsignalized access to US-11 for several businesses.

Watterson Parkway is a two-lane roadway within the study area connecting US-11 to Chalkville Road. It has a posted speed limit of 30 MPH. The high-clearance Watterson Parkway bridge extends over Pinchgut Creek and the railroad tracks.

Chalkville Road is a two-lane, urban collector roadway oriented north to south within the study area. It has a posted speed limit of 30 MPH. According to the ALDOT Traffic Data website, the 2021 AADT volume along Chalkville Road was 12,359 vehicles per day between US-11 and Watterson Parkway. Approximately 770 feet south of the intersection of US-11 at Chalkville Road is the railroad crossing over Pinchgut Creek.

Railroad Avenue is a two-lane local roadway within the study area. It has a posted speed limit of 30 MPH and is oriented east to west within the study area. Railroad Avenue connects Chalkville Road to City Hall Drive and the municipal campus just north of the railroad tracks. Approximately 800 linear feet of sidewalk exists along Railroad Avenue between Chalkville Road and Bryant Bank.

Parkway Drive is a two-lane, local roadway intersecting US-11 adjacent to the Trussville Veterans Memorial at Civitan Park. The intersection of Parkway Drive and US-11 is controlled by a traffic signal.

Several other local streets comprising the downtown grid south of US-11 include Beech Street, Cedar Street, Railroad Street, and City Hall Drive.

2.3 Existing Parks and Recreational Areas

Parks and recreational facilities often represent sufficient logical termini for sidewalks, bicycle facilities, trails, and other active transportation accommodations. A trail within the study area could provide pedestrian and bicycle access to Civitan Park and Trussville Athletic Complex from businesses and amenities downtown. Within Civitan Park, the Trussville Veterans Memorial is located on the northeast quadrant of the intersection of US-11 at Parkway Drive. It has a paved trail system, which crosses the Cahaba River and runs north along the riverside to connect Civitan Park and the Trussville Athletic Complex.

Photo 5 displays an example of a path near the Veterans Memorial.



Photo 5: Concrete Path near Veterans Memorial at Civitan Park

Figure 3 shows the location of the parks relative to downtown Trussville.



Figure 3: Existing Parks, Recreational Facilities, and Trail Systems

2.4 Previous Documents and Adjacent Projects

Several documents were reviewed to evaluate the existing conditions of the study area. This section summarizes the documents that were reviewed.

RPCGB Long-Range Transportation Plan

This document was reviewed to determine if any upcoming projects nearby could affect the Pinchgut Creek trail study. The projects in the Trussville area included in the Long-Range Transportation Plan include:

- US-11 (from Chalkville Road to Cahaba River) – Widen from Four to Five Lanes
 - FY 2019 – This project is in the latter stages of construction at the time of this study.

Trussville 2040 Active Transportation Plan

This plan represents pedestrian, non-motorized personal transport (bicycles and skateboards), electrified personal transport (E-bikes), wheelchairs, and electric scooters. It places emphasis on outdoor recreation, parks, and the preservation of natural open spaces, along with designing a well-functioning active transportation network around these elements. The Active Transportation Plan includes several recommendations for an active transportation network in the Trussville area:

- Greenway extension from Civitan Park, connecting to Downtown and along Pinchgut Creek
- Greenway extension from Civitan Park north along the Cahaba River toward Stockton.
- Multi-use path connection across US-11 to the future Greenway extension.
- Development of a park and trailhead at property adjacent to Winn Dixie.
- Planned trailhead locations that provide access to a major network throughout Trussville, designed to give access to all residents of Trussville and help shape future development.

Trussville Municipal Campus

Civil Consultants, Inc. were contracted by the City for master planning and design of the Trussville Municipal Campus area between City Hall Drive and the Cahaba River. This campus includes Trussville City Hall, Trussville Police Department, and Trussville Gas and Water.

Sain Associates coordinated with Civil Consultants, Inc. regarding the segment of the study area between City Hall Drive and Trussville Veterans Memorial at Civitan Park. At the time of this study, this project was in the design stage with plans to incorporate a proposed trail through the municipal campus and across US-11. Options for crossing US-11 include an at-grade crossing at Parkway Drive or utilizing the space under the US-11 bridge over the Cahaba River.

2.5 Field Observations

Field reviews were performed on Tuesday, September 27, 2022, and October 11, 2022, by Sain Associates and PLOT Studio. The goals of the field review were to identify existing infrastructure and utilities, assess existing bicycle and pedestrian accommodations, and establish potential alignments for new location trail along Pinchgut Creek. After potential alignments were established, the constructability and ADA-accessibility were considered for each section of the study area.

Section A: Veterans Memorial at Civitan Park to City Hall Drive

Section A begins at Civitan Park, specifically near the Veterans Memorial. The ongoing Trussville Municipal Campus project will result in changes within the boundary of US-11, City Hall Drive, the railroad tracks, and the Cahaba River. As noted in Section 2.4, this project is in the design stage and coordination has been performed with the City and its consultants on the project to maintain continuity with this study.

The main challenge in this segment is connecting the municipal campus to Veterans Memorial at Civitan Park. The most feasible options include an at-grade crossing of US-11 at Parkway Drive or a connection under the US-11 bridge over the Cahaba River. There is space between the bridge bents that house a gas line, but its possible a connection could be made alongside it. **Photo 6** shows a view of the space under the bridge. If a connection is constructed under the bridge, it is likely that a net would be required above the trail on the underside of the bridge structure to catch any potential falling debris.



Photo 6: View of US-11 bridge over the Cahaba River (Potential Trail Location)

Section B: City Hall Drive to Trussville Entertainment District

Section B includes several downtown streets, Chalkville Road, and the Trussville Entertainment District. Beginning at City Hall Drive, the grassed area between the Bryant Bank parking lot and the railroad tracks provides space on the south side of Railroad Avenue. The grassed area is private property, and right-of-way acquisition would be required. At the intersection with Railroad Street, the trail would likely follow the existing sidewalk on the north side of Railroad Avenue to Chalkville Road. **Figure 4** shows an aerial view of Railroad Avenue and the surrounding area.

The existing sidewalk along Railroad Avenue is 4 feet wide and would require improvement to accommodate the trail. Coordination with any redevelopment of the parcels on the north side of Railroad Avenue would be required to address any right-of-way concerns.



Figure 4: Railroad Avenue Area

Drainage accommodations along Railroad Avenue vary. Traditional curb and gutter transitions to valley gutter in some locations. Several drainage inlets and at least one culvert exist along Railroad Avenue. Power lines, gas lines, and sanitary sewer lines lie adjacent to or cross Railroad Avenue in several locations.

Challenges in Section B include utility conflicts, right-of-way acquisition, and providing a bicycle and pedestrian crossing at Chalkville Road. If improvements cannot be made to the sidewalk to accommodate bicycles, Railroad Avenue is a low-stress route for cyclists that would be an appropriate candidate for shared lanes or “sharrows.”

Photo 7 shows the Chalkville Road bridge over Pinchgut Creek, located just north of the railroad crossing. There are major constructability and environmental concerns with routing the trail under the bridge, and it's likely that this is not a feasible route for the trail. A more feasible alternative is an at-grade crossing of Chalkville Road just north of Railroad Avenue. **Photo 8** shows Chalkville Road as it approaches US-11.



Photo 7: Pinchgut Creek under Chalkville Road



Photo 8: Chalkville Road between US-11 and Pinchgut Creek

Section C: Trussville Entertainment District to Winn Dixie

The Trussville Entertainment District is located on the west side of Chalkville Road just south of US-11. Several restaurants, a brewery, and an event venue stage offer a destination for residents. A bicycle and pedestrian connection along the creek would provide benefits in relieving the limited supply of parking at night and on weekends. **Photo 9** shows a birds-eye view of the entertainment district area.



Photo 9: Birds-Eye View of the Trussville Entertainment District and Surrounding Area

Challenges in this area include constructability, environmental impacts, and utility impacts. Constructing a trail directly on the grassed slope between the entertainment district and Pinchgut Creek presents concerns about impacts to the creek. An elevated structure or boardwalk may be required for up to 500 feet to eclipse drainage structures, account for grade differences, and avoid direct impacts to Pinchgut Creek. **Photo 10** shows the area behind the event stage at the Trussville Entertainment District.



Photo 10: Grassed Area between Entertainment District and Pinchgut Creek

Photos 11 and 12 show drainage structures located between the entertainment district and Pinchgut Creek. The pipe in **Photo 11** is located on the rear side of the stage, and the headwall in **Photo 12** is located near the loading docks just west of the stage. Both pipes flow directly into Pinchgut Creek during rainfall events.



Photo 11: Drainage Pipe under Trussville Entertainment District Stage



Photo 12: Headwall near Entertainment District Loading Docks

On the west of the Trussville Entertainment District, a connection to the sidewalk network could be made. The opportunity to create a trailhead adjacent to the existing parking lot is also an option with the additional space between the parking lot and Pinchgut Creek.

Between the entertainment district and Watterson Parkway, there is a large drainage channel that would require a structure to span it. **Photo 13** shows a birds-eye view of this location, and **Photo 14** shows the drainage channel itself.



Photo 13: Birds-eye View of the Area between the Entertainment District and Watterson Parkway



Photo 14: Drainage Channel Terminus at Pinchgut Creek

As shown in **Photo 14**, there is sufficient clearance below the Watterson Parkway overpass for the proposed trail to cross under. West of the Watterson Parkway overpass, the terrain becomes more uniform in a joint easement between the U.S. Army Corps of Engineers (USACOE) and Jefferson County. The USACOE easement is for Pinchgut Creek itself, while the Jefferson County easement is for a sanitary sewer line. Further discussion of the easement can be found in Section 3.8 of this report. **Photo 15** shows a typical view of the easement running along the north side of Pinchgut Creek.



Photo 15: Typical View of the Easement along Pinchgut Creek in the Study Area, Looking Southwest

The easement consists of Pinchgut Creek itself, the creek bank, a relatively flat segment used by maintenance vehicles, and a spoil bank. **Photo 16** shows an overhead view of the easement along Pinchgut Creek. In general, the worn vehicle path along the easement provides a good initial location for the proposed trail. Two large culverts exist along this segment of the easement to facilitate additional drainage from the north.



Photo 16: Birds-eye View of Pinchgut Creek Easement

Just south of the Winn Dixie shopping center, the cleared and maintained area diverts northwest from the easement and Pinchgut Creek and terminates on the southwest corner of the Winn Dixie shopping center parking lot. Another drainage channel follows this cleared area from the parking lot towards Pinchgut Creek. The location of this drainage channel is shown in **Photo 17**.



Photo 17: Easement with reference to the Winn Dixie Shopping Center

At the fork in the cleared area shown in **Photo 17**, the proposed trail could diverge from Pinchgut Creek towards the Winn Dixie parking lot where an opportunity for another trailhead exists. **Photo 18** shows this location.



Photo 18: Headwall and Drainage Channel at Southwest Corner of Winn Dixie Parking Lot

Most of the easement along Pinchgut Creek is in the 100-year floodplain. The worn vehicle path throughout the easement was dry at the time of the field visit.

Section D: Winn Dixie to Homestead Village Shopping Center

An undeveloped, heavily wooded area lies between the Winn Dixie shopping center and the Homestead Village Shopping Center, which was constructed in 2016. The parcels in this section are mostly covered by the 100-year floodplain. To make a connection between the proposed trail in Section C and the Homestead Village Shopping Center, a structure would be needed to span the drainage channel on the east side and a grade differential on the west side of the section. The drainage channel connecting the Winn Dixie parking lot and Pinchgut Creek is shown in **Photo 19**.



Photo 19: View of the Drainage Channel Separating Sections C and D

The USACOE easement continues approximately 700 to 800 feet into Section D, but it is uncleared. The sanitary sewer easement continues along the north side of the creek to Homestead Village Shopping Center.

The wooded parcels within Section D are relatively flat. However, there are steep slopes surrounding the Homestead Village Shopping Center. A wide drainage ditch carries water around the site toward Pinchgut Creek. Challenges in this section include environmental permitting, grade differentials near Homestead Village Shopping Center, and utility impacts. **Photo 20** shows a birds-eye view of the Homestead Village Shopping Center.

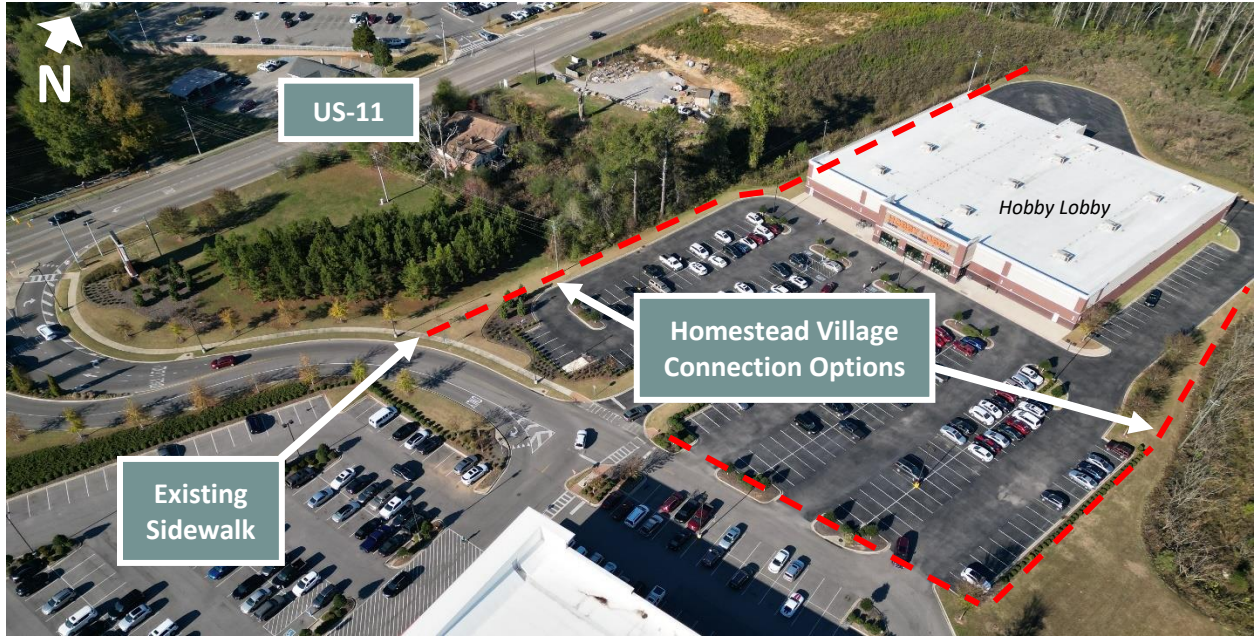


Photo 20: Birds-eye View of the Homestead Village Shopping Center

The Homestead Village Shopping Center represents the western terminus of the study area. Existing sidewalk along the access road to US-11 presents a logical terminus for the proposed trail. There are two alternatives for potential connections in this area: along the northwestern edge of the parking lot or along the southeastern edge of the parking lot. In either case, grade differential is considerable between the parking lot and the wooded area of Section D. However, slopes along the southeastern edge of the parking lot appear to be steeper than the northwestern edge of the parking lot.

Utility impacts will be inevitable with either option at Homestead Village Shopping Center. On the northwestern edge of the parking lot, there are gas, power, and water utility infrastructure that would need to be relocated or avoided. On the southeastern edge of the parking lot, storm sewer and sanitary sewer lines would be impacted. **Photos 21 and 22** show examples of the utilities in the vicinity of Hobby Lobby.



Photo 21: Utilities along Northwestern Edge of Hobby Lobby Parking Lot



Photo 22: Utilities along the Southwestern Edge of Hobby Lobby Parking Lot

The logical terminus on the western end of the study area is the existing sidewalk along the Homestead Village Shopping Center access road. The sidewalk is a 6 feet wide concrete path connecting to the storefronts via internal crosswalks. The existing sidewalk is shown in **Photo 23**.



Photo 23: Existing Sidewalk at Homestead Village Shopping Center

Connecting to the sidewalk network at this location would also provide access for pedestrians to Tutwiler Drive and other retail stores on the northwest side of US-11. Crosswalks and pedestrian signal heads exist at the Homestead Village Shopping Center access's intersection with US-11 and Tutwiler Drive.

3 Environmental Features

A search of documents, databases, a field review, and compilation of GIS data was performed to analyze existing conditions and identify environmental features. This section further discusses the gathered data.

3.1 Historic Assets

A search of various databases was performed to identify any known historic properties. This section summarizes this research. To verify the historic and/or archeological importance of the area, a full cultural resources report should be prepared should the City opt to utilize federal monies to fund improvement projects within this area.

The National Register of Historic Places (NRHP)

A search of the NRHP reveals one property listing in the Trussville Area. This property, the Cahaba Homestead Village Historic District was entered in the National Register in March of 2006 due to its local significance. There are 266 contributing resources and 11 non-contributing resources included in the district. As listed in the NRHP registration form the Cahaba Homestead Village Historic District is contained within the boundaries of Brentwood Avenue that runs into Rockridge Avenue, Poplar Street, and Parkway Drive. The streets are arranged in a grid pattern with Poplar, Pine, Oak, Lake, Magnolia Streets and Magnolia Court oriented in an approximate west/east direction and Brentwood and Rockridge Avenues, Chalkville Road, and Parkway Drive in an approximate north/south direction. Although this NRHP listed district is located relatively close to the study area, it is not included in the study area.

Alabama Register of Landmarks and Heritage (ARLH)

There are no properties within the study area recorded on the Alabama Register of Landmarks and Heritage. However, there are four Trussville properties listed on the ARLH including the Happy Hollow District Bridge located in Civitan Park near the eastern end of the study area. The bridge, which is shown in **Photo 24**, is located approximately 100 yards north of US-11 over the Cahaba River.

Additionally, there is a historical marker located at the intersection of US-11 (Main Street) and Chalkville Road. This marker was erected in 1995 by the Trussville Historical Board and recognizes the Confederate storehouse that was burned by federal troops on April 20, 1865.



Photo 24: Happy Hollow District Bridge at Civitan Park

Alabama Historic Cemetery Register (AHR)

There are no cemeteries within the study area recorded on the Alabama Historic Cemetery Register. However, the nearby Trussville Memorial Cemetery is located on the north side of US-11 (Main Street) between Cherry Avenue and Parkway Drive. The cemetery was listed in the AHR on November 3, 2016 and the Trussville Memorial Cemetery Association erected a historical marker there in 2020.

3.2 Section 4(f) Properties

The U.S. Department of Transportation Act of 1996 included a Section 4(f) which provided for consideration of park and recreation areas, wildlife and waterfowl refuges, and historic sites during transportation project development. Although this Act is now implemented by the Federal Highway Administration (FHWA) through the regulation 23 Code of Federal Regulations (CFR) 774, Section 4(f) is still used to describe the above-mentioned property types. Although there are historic and public recreational properties located near the study area there are no Section 4(f) properties located within the study area.

3.3 Threatened and Endangered Species

A search using the US Fish and Wildlife Service's IPaC Information for Planning and Consultation database was performed, and the resulting resource list is included in

Appendix C. The search provided a list of known or expected threatened or endangered species located within the study area. In all, IPaC lists sixteen (16) species that could be potentially impacted by activities in the study area. **Table 1** provides a listing of the species and associated status. Per USFWS IPaC, there are no critical habitats located in the study area.

Table 1: USFWS IPaC Results

Name	Status
Gray Bat	Endangered
Indiana Bat	Endangered
Northern Long-eared bat	Threatened
Alligator Snapping Turtle	Proposed Threatened
Cahaba Shiner	Endangered
Alabama Moccasinshell	Threatened
Finelined Pocketbook	Threatened
Orangenacre Mucket	Threatened
Ovate Clubshell	Endangered
Southern Clubshell	Endangered
Southern Pigtoe	Endangered
Triangular Kidneyshell	Endangered
Upland Combshell	Endangered
Monarch Butterfly	Candidate
Georgia Rockcress	Threatened
Tennessee Yellow-eyed Grass	Endangered

The first three species named by IPaC are bats. The presence of bats impacts clearing windows due to roosting times. Construction related to culvert removal, etc. can also be impacted by the presence of bats. In addition to the sixteen (16) species listed in **Table 1**, there are eight (8) species of migratory birds that could be found in the study area. These species are protected under the Migratory Bird Treaty Act.

Based on the information provided by the USFWS IPaC, a Threatened and Endangered Species survey would be required should the City opt to utilize federal monies to fund improvement projects within this area. A species study would likely be required for USACE permitting as well.

3.4 Streams and Wetlands

The study area follows Pinchgut Creek, which is a riverine habitat shown in **Photo 25**. According to the National Wetlands Inventory (NWI), the study area includes Palustrine, freshwater emergent wetland areas. Mapping of these areas is included in the IPaC report in **Appendix C**. A streams and wetlands jurisdictional determination study will likely

be required if Pinchgut Creek is traversed or impacted by the proposed trail. Impacts to jurisdictional streams and wetlands could require a USACE permit.

The proposed trail is anticipated to have minor impacts to the Pinchgut Creek floodplain. The area experiences flash flooding during high volume rainfall events. Based on local officials' input, downtown Trussville has sustained catastrophic damage in two separate events since 2000.



Photo 25: View of Pinchgut Creek

3.5 Prime and Unique Farmlands

For highway projects using federal funds, the Farmland Protection Policy Act applies. This means for federal highway projects that have the potential to convert important farmland to a non-farm use, the land must be evaluated using the NRCS's LESA system. This land evaluation and site assessment system establishes a farmland conversion impact rating score, and this score is used to determine if potential adverse impacts on the farmland exceed the recommended allowable level.

A search of the Natural Resources Conservation Service's (NRCS) Web Soil Survey was used to determine the potential for prime and unique farmlands within the study area. Mapping generated by the Web Soil Survey website is provided in **Appendix D**. The NRCS Web Soil Survey shows that the study area contains some prime farmland; however, the current land use for this area does not include farming but residential and municipal use.

Should the proposed trail be implemented with federal funds an AD-1006, Farmland Conversion Rating form will be required. The site assessment portion of this form which assesses non-soil related criteria, is completed by the sponsoring agency and the USDA makes the final determination.

3.6 Hazardous Materials Properties

A search of the Alabama Department of Environmental Management's (ADEM) GIS Inspector tool was performed. There are a few UST incident sites in the vicinity of the study area; however, these sites have a cleanup date. Other than these cleared locations and the wastewater treatment facility, there are no known hazardous materials sites located within the study area. Should the City elect to move forward with improvements using Federal or State money, a hazardous materials clearance letter will have to be obtained from ALDOT's Environmental Technical Section (ETS).

3.7 Environmental Justice

Environmental Justice (EJ) is a component of the National Environmental Policy Act (NEPA) that seeks to ensure that all socio-economic groups share in the benefits and burdens of Federal transportation projects. Two areas of environmental justice that frequently become a concern are areas with a high minority population or areas where the majority of the inhabitants are members of low-income households.

Table 2 provides a brief overview of the socioeconomic demographics surrounding the study area. The data is from the Environmental Protection Agency's (EPA) Environmental Justice Screening Tool (EJ Screen). The EJ Screen report is included in **Appendix E**. A 0.50-mile buffer area was added to the proposed trail path as that is a typical buffer used for FHWA projects.

Table 2: Environmental Justice Screen Tool Summary

Socioeconomic Overview	Value	State Average	Percentile in State	EPA Region Average	Percentile in EPA Region	USA Average	Percentile in USA
Demographic Index	15%	36%	15	37%	14	36%	20
People of Color	13%	34%	28	39%	24	40%	26
Low Income	17%	37%	17	35%	20	31%	30
Linguistically Isolated	2%	1%	84	3%	67	5%	60
Less Than High School Education	5%	14%	20	13%	23	12%	28

When compared to data for the state, the EPA region, and the country, it can be concluded that there are no concerns related to environmental justice. The minority population percentages of the area are below what is seen in the state, the EPA region, and the country. The percentage of families living below the poverty line is also below that of the state, the EPA region, and the country.

If federal monies are used to install improvements, the sponsoring agency will need to ensure that all planning and outreach components of the project comply with environmental justice regulations under NEPA. Adverse effects to EJ communities are unlikely due to the nature of the proposed improvements centering on pedestrian mobility, but the proper steps outlined by the EPA for community involvement on projects should be followed.

3.8 Utilities

Various utilities and easements exist within the study area. Overhead power, water, sanitary sewer, and gas utilities can be found in the downtown grid, entertainment district, and shopping centers. The aforementioned United States Army Corps of Engineers (USACOE) right-of-way for Pinchgut Creek runs concurrently with a Jefferson County sanitary sewer easement. A map of parcel data and sanitary sewer line location can be found in **Appendix B**.

The USACOE right-of-way consists of a 110-foot wide permanent easement running from the Cahaba River to mid-way through Section D of this study. In addition to the 110-foot wide permanent easement, a 65-foot wide temporary easement was included on the north side of the creek to accommodate a spoil bank in plans for the Pinchgut Creek channel. Plans are not dated, but deeds and condemnation documents indicated that the project was undertaken in the mid-1960's. **Figure 5** displays the typical section for the easement.

The Jefferson County sanitary sewer easement is located within the USACOE right-of-way, and the manholes can be seen throughout the length of the easement.

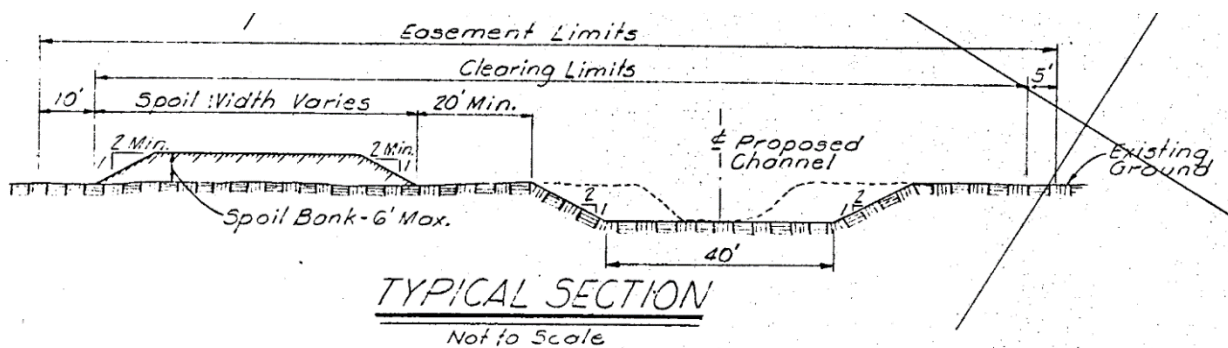


Figure 5: USACOE Pinchgut Creek Right-of-Way Typical Section

Photo 26 shows an overhead view of the easement in the vicinity of Watterson Parkway, as it appears at the time of this study.



Photo 26: Overhead View of the USACOE-Jefferson County Easement

4.1 Section A: Veterans Memorial at Civitan Park to City Hall Drive

The proposed improvements for Section A include implementing a 10-foot wide multi-use trail through the City's municipal campus. The proposed 10-foot wide concrete path would begin at the existing concrete paths near Veterans Memorial at Civitan Park and cross under US-11 on the west side of the Cahaba River bank to connect to the municipal campus. The proposed trail would continue along the southern edge of the parking lot toward City Hall Drive. **Figure 7** contains a diagram of proposed improvements within Section A.

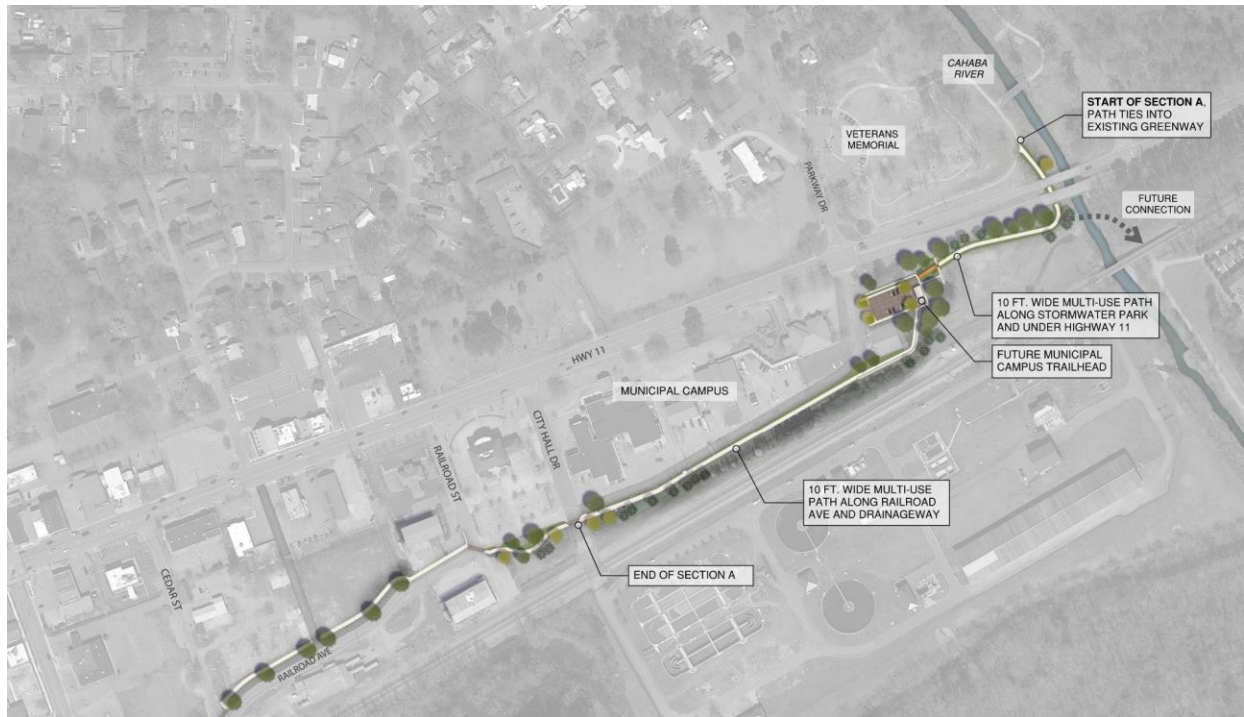


Figure 7: Section A Conceptual Diagram of Proposed Improvements

The following improvements are proposed within Section A of the study area, as shown in **Figure 7**:

1. Construct a 10-foot wide path with concrete or boardwalk surface from Veterans Memorial at Civitan Park to the intersection of City Hall Drive and Railroad Avenue.
2. Install a crosswalk on the southern leg of the City Hall Drive at Railroad Avenue intersection.

As discussed in Section 2.3 of this report, the improvements recommended for Section A would be constructed as a part of the ongoing Trussville municipal campus project. This project is located east of City Hall Drive between US-11, the Cahaba River, and the railroad tracks.

4.2 Section B: Trussville Entertainment District to City Hall Drive

The proposed improvements for Section B include implementing a 10-foot wide multi-use trail along the north side of Railroad Avenue, across Chalkville Road, and behind the Trussville Entertainment Center before connecting with Section C. Along Railroad Avenue, the proposed trail would require improving existing sidewalk and constructing a boardwalk structure between the entertainment district stage and Pinchgut Creek. **Figure 8** contains a diagram of proposed improvements within Section B.

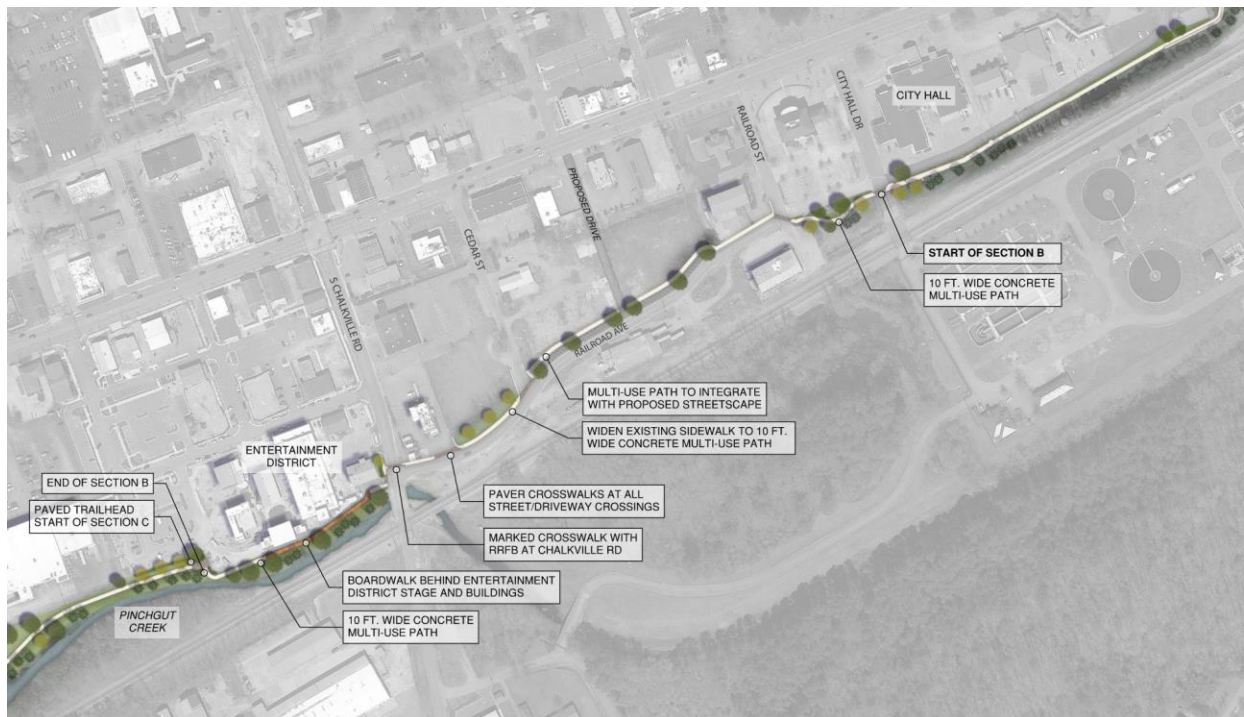


Figure 8: Section B Conceptual Diagram of Proposed Improvements

The following improvements are proposed within Section B of the study area, as shown in **Figure 8**:

1. Remove and replace the existing sidewalk along the north side of Railroad Avenue to a width of 10 feet. Install new sidewalk to connect the existing sidewalks on Railroad Avenue and Chalkville Road.
2. Install a rectangular rapid flashing beacon (RRFB) and marked crosswalk on the northern leg of the Chalkville Road intersection with Railroad Avenue.
3. Construct a boardwalk structure from the former Golden Rule BBQ site to the southwest corner of the Trussville Entertainment District, just north of Pinchgut Creek. Install a 10-foot wide concrete path on either end of the boardwalk to connect it to the Chalkville Road crosswalk and proposed entertainment district trailhead in Section C.

Figure 9 shows a rendering, or section perspective, of the proposed trail along Railroad Avenue between Chalkville Road and Railroad Street. **Figure 10** displays examples of trail furnishings and lighting that would be typical for all sections of the trail.



Figure 9: Railroad Avenue Section Perspective



Figure 10: Typical Trail Furniture and Lighting (All Sections)

Figure 11 displays two renderings of the proposed trail in the area between Pinchgut Creek and the Trussville Entertainment District, just west of Chalkville Road. Existing drainage structures could remain and flow under the boardwalk, allowing riparian planting to continue to establish itself on the creek bank.



Figure 11: Section Perspectives for Proposed Trail at Trussville Entertainment District

4.3 Section C: Trussville Entertainment District to Winn Dixie Shopping Center

The proposed improvements for Section C include implementing a 10-foot wide multi-use trail along the north side of Pinchgut Creek from the Trussville Entertainment District to the Winn Dixie Shopping Center. The proposed trail should follow the USACOE-Jefferson County easement for approximately 0.5 miles before diverting northwest from the creek towards the Winn Dixie parking lot. **Figure 12** contains a diagram of proposed improvements within Section C.



Figure 12: Section C Conceptual Diagram of Proposed Improvements

The following improvements are proposed within Section C of the study area, as shown in **Figure 12**:

1. Construct a 10-foot wide concrete path along the north side of Pinchgut Creek from the Trussville Entertainment District's southwest corner to Winn Dixie Shopping Center's southwest corner. For the subsection of proposed trail along the Pinchgut Creek easement, the path should be constructed at the base of the existing spoil bank where terrain flattens out. Where required, construct a structure for the trail to span the drainage channel just east of Watterson Parkway.
2. Construct a trailhead at each end of Section C. One trailhead is proposed adjacent to the Trussville Entertainment District, and a second trailhead is proposed at the southwest corner of the Winn Dixie Shopping Center's parking lot.

Figure 13 shows a rendering of the proposed trail along Pinchgut Creek within Section C. The proposed trail would sit within the USACOE-Jefferson County utility easement in the

level terrain between the creek bank and the existing spoil bank in the easement. **Figure 14** shows an example of a proposed trailhead, which has multiple applications within the study area at the Winn-Dixie parking lot, the Trussville Entertainment District, and the municipal campus.

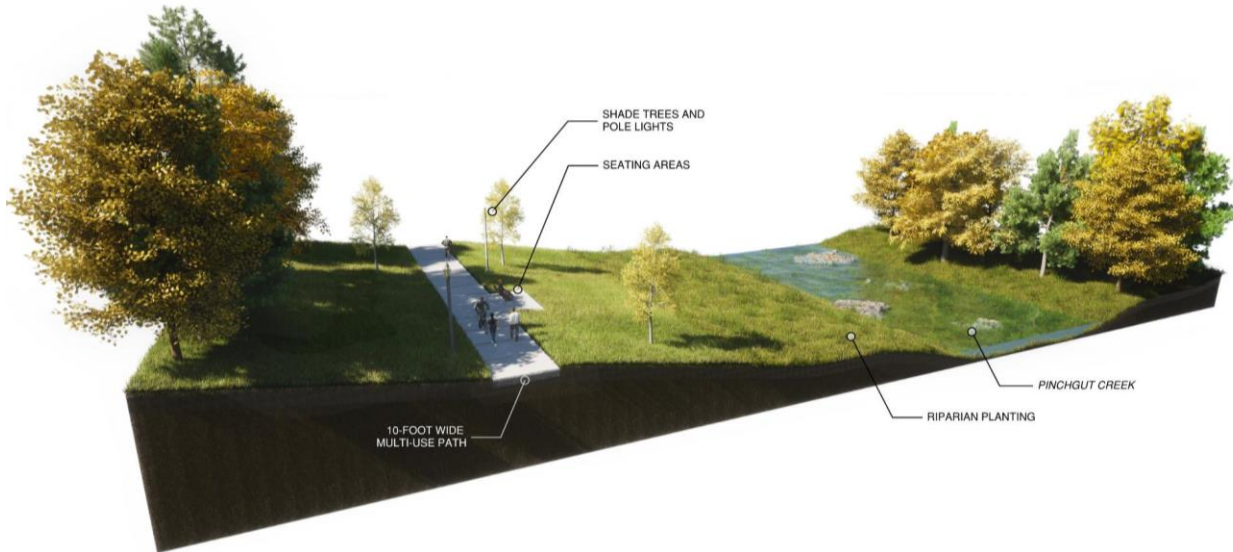


Figure 13: Section Perspective along Pinchgut Creek between Winn-Dixie and Watterson Parkway



Figure 14: Section Perspective of Proposed Trailhead at Winn-Dixie Parking Lot

4.4 Section D: Winn Dixie Shopping Center to Homestead Village Shopping Center

The proposed improvements for Section D include installation of a 10-foot wide path along the north side of Pinchgut Creek from the Winn Dixie Shopping Center to the Homestead Village Shopping Center. Commercial establishments and existing sidewalk within the Homestead Village Shopping Center provides a logical terminus for the proposed trail. **Figure 10** contains a diagram of proposed improvements within Section D.

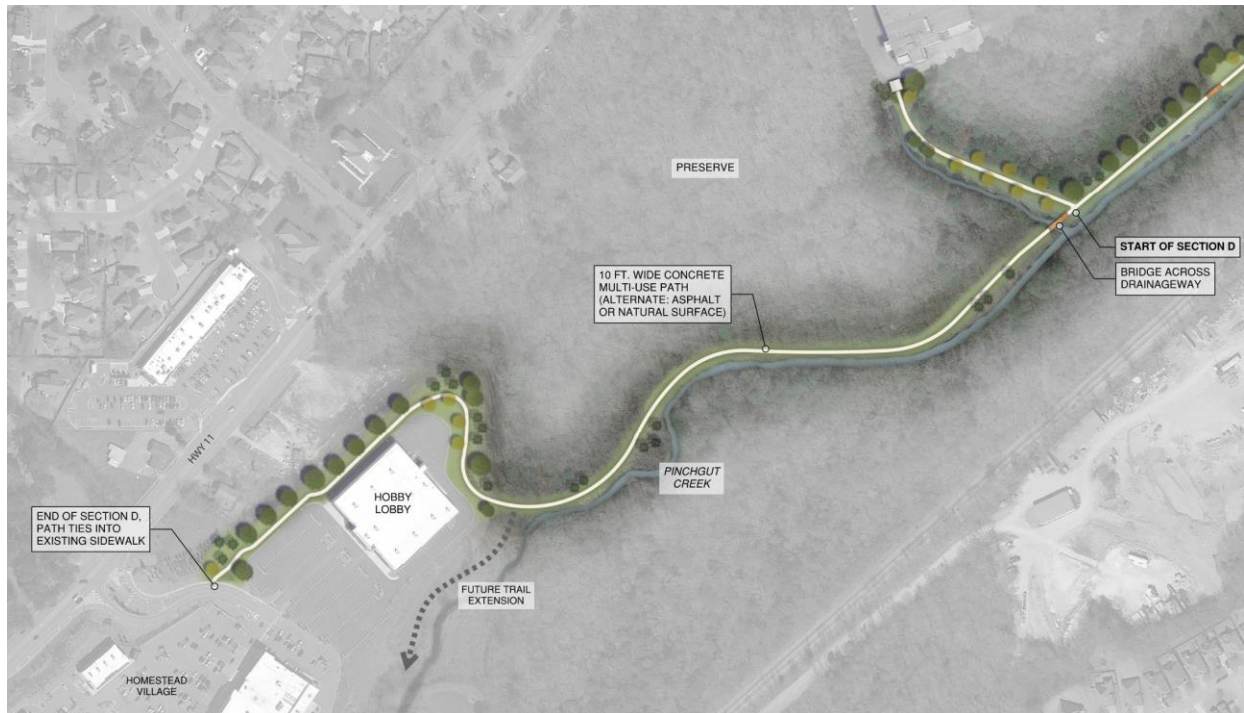


Figure 15: Section D Conceptual Diagram of Proposed Improvements

The following improvements are proposed within Section D of the study area, as shown in **Figure 10**:

1. Construct a 10-foot wide concrete path along the north side of Pinchgut Creek from the Section C path to the existing sidewalk within the Homestead Village Shopping Center.
2. Due to the environmentally sensitive nature of the parcels within Section D, asphalt or natural surface trail may be necessary instead of concrete. Reevaluate during the design phase and adjust plans as necessary.
3. If the grade differential between the Homestead Village Shopping Center parking lot and the low-lying areas of Section D render ADA-compliance infeasible, install adequate signage at each end of proposed trail for Section D to warn trail users.

4.5 Opinions of Probable Cost

All costs included in this report are planning level costs and assume the project utilizes only local funding to install the improvements. Roadway items including sidewalk were estimated based on engineering experience and bid tabulations from recent projects. The total estimated project cost was prepared for the 2023 planning year. This number should be increased to account for rising costs due to inflation should the improvements not be implemented in 2023.

The totals include opinions of probable cost of construction, preliminary engineering (17%), utility relocation, right-of-way, and a 20% contingency. The City aims to fund the project locally. Therefore, construction engineering and inspection (CE&I) and ALDOT indirect costs are not included in the opinions of probable cost. Some level of CE&I is recommended for the project, but the cost of CE&I on a locally funded project would be less than the typical cost of an ALDOT or federally funded project.

Table 3 provides a summary of opinions of probable cost for each section of improvements, with the assumption that the project is funded locally. **Appendix F** contains detailed estimates of opinions of probable cost.

The proposed improvements within Section A are planned to be constructed as a part of the City's municipal campus project, which is in the design stage at the time of this study. For that reason, an opinion of probable cost is not included for Section A. The proposed improvements within Sections B & C would likely be constructed within one project, with the proposed improvements in Section D progressing along a different timeline.

Table 3: Opinions of Probable Cost Proposed Improvements (Local Funds)

Improvement Section	Opinion of Probable Cost
Section A	-
Section B & C	\$2.7 million
Section D	\$1.2 million
Total (Sections B, C, & D)	\$3.9 million

*If project is funded with state or federal funds, these costs will increase.

Utility relocation costs are included in this estimate. However, utility costs are highly variable and dependent on several factors. Due to limited information on the existing utilities at the time of this estimate, the cost provided should be considered as budgetary only. Preliminary Engineering includes environmental, survey, geotechnical, traffic, and transportation design only. Contingency includes miscellaneous and/or unknown items that cannot be quantified at the time this study was conducted. The cost of trail furniture such as benches, trash cans, and decorative signage are not included in the totals.

5 Accessibility

Per the Americans with Disabilities Act (ADA), facilities located within the public right-of-way must provide accessibility for all users including those with disabilities. The United States Access Board has developed proposed guidelines for pedestrian facilities in public rights-of-way. These guidelines are more commonly referred to as *Public Rights-Of-Way Accessibility Guidelines* or PROWAG. Per PROWAG, design, construction, and any alteration of pedestrian facilities within public rights-of-way, including local rights-of-way, must be made accessible for pedestrians with disabilities. Although PROWAG has not yet been officially adopted by the United States Department of Justice, it is the standard recognized by ALDOT. Once PROWAG is officially adopted, it will be mandatory that the guidelines set forth by the United States Access Board be implemented into projects located within public rights-of-way.

Sections A, B, and C should be designed in a manner that meets ADA and PROWAG guidelines. Grade differences and unique terrain in Section D could be problematic in adhering to ADA and PROWAG guidelines. In the event that it becomes infeasible to meet ADA and PROWAG guidance for Section D, adequate signage should be posted notifying facility users of the end of ADA accessible accommodations within Section D.

6 Funding Sources

Costs associated with the design and construction of the proposed improvements could exceed the City's current available resources. This section discusses funding sources that are available to aid in design and construction. Federal programs are administered by the Alabama Department of Transportation. **Table 4** details funding sources, the category of the source and type of project for which the funding can be used.

Table 4: Funding Options

Funding Source	Category	Match Type
Transportation Alternatives Program (TAP)	Federal	80% Federal / 20% City
Recreational Trail Program (RTP)	Federal	80% Federal / 20% City
Rebuild Alabama Act – Annual Grant Program	State	Up to 100% State
Local Municipality	Local	100% Local

6.1 Federal Funding

The **Transportation Alternatives Program (TAP)** is an 80% Federal/20% Local match program continued through the Infrastructure Investment and Jobs Act (IIJA). TAP funding is available for projects defined as transportation alternatives. Example of transportation alternatives include the following scenarios: on- and off-road pedestrian and bicycle facilities, infrastructure projects for improving non-driver access to public transportation and enhance mobility, community improvement activities such as historic preservation and vegetation management, environmental mitigation related to stormwater and habitat connectivity, recreational trail projects, safe routes to school projects, and projects for planning, designing, or constructing boulevards and other roadways largely in the right-of-way of former divided highways. Tap funding can be pursued through an application with RPCGB and ALDOT. The same application can be used for submittal to ALDOT and RPCGB. The application deadline is typically due in early summer of each year. The total amount of federal participating funding for a project is capped at \$1,000,000 (\$800,000 federal/\$200,000 local match).

The **Recreational Trail Program (RTP)** is a federal competitive grant program that is administered by the Alabama Department of Economic and Community Affairs (ADECA). During the FY2023 Funding Cycle, a total of \$1.6 million was appropriated for the program. The RTP allowed varying maximum grant amounts based on the trail type (\$200,000 for non-motorized, single-use trails; \$400,000 for non-motorized, diverse-use trails). Applicable permissible uses include the following: development of urban trail linkages; development of trailside and trailhead facilities; acquisition of easement for trail use; and construction of new trails. Applications are typically due in the fall each year, and consultation with ADECA staff is required prior to submitting an application. Applicants with an open Land and Water Conservation Fund (LWCF) or unresolved compliance issues from a previous grant may be ineligible to apply for this funding source.

6.2 State Funding

The Rebuild Alabama Act authorizes the **ALDOT Annual Grant Program**, a \$10 million fund, for which cities and counties may apply. Additionally, the Program provides the opportunity for cities and counties to partner with the State on larger projects where adequate local funding may not be available. There is not a specified or required match for local governments to take on, but any funds that local governments can leverage to team with ALDOT to fund a project could play a role in the decision-making process. Up to \$250,000 per project can be awarded from this fund, and funds must be used for construction or a federal match when construction is imminent.

6.3 Local Funding

The City has the option to fund the design and construction of their preferred alternative using only local funds. Choosing this route allows the project design and construction to

have shorter timelines and the potential for reduced project costs since fewer plan reviews would be required and City guidelines will govern the project design. The timeline for a locally funded project is estimated at 2-4 years.

7 Stakeholder Involvement

A kickoff meeting was held on June 29, 2022, at Trussville City Hall to discuss the project background, identify study priorities, and determine expectations for the study. Representatives from the City of Trussville, Regional Planning Commission of Greater Birmingham (RPCGB), and Sain Associates were present.

A second meeting was held on November 16, 2022, at the Trussville City Hall Annex to discuss findings from existing conditions, preliminary trail alignments, deliverables, and next steps for the study. Representatives from the City of Trussville, Regional Planning Commission of Greater Birmingham (RPCGB), PLOT Studio, and Sain Associates were present.

A third meeting was held on April 4, 2023, at the Trussville City Hall Annex to present the results of the study to representatives from several local businesses in the study area. Feedback from the local business representatives was positive towards the study results. Representatives from the City of Trussville, Regional Planning Commission of Greater Birmingham (RPCGB), PLOT Studio, and Sain Associates were present.

8 Next Steps

The City has stated a preference for using local funds to construct the project. If locally funded, the timing, scheduling, and implementation of the installation would be at their discretion.

If instead the City chooses to move forward with implementing any portion of the proposed improvements with federal funding, the next step would be to request inclusion of a project in RPCGB's Transportation Improvement Plan (TIP) for Fiscal Years 2024 – 2027. The preparation of this feasibility study can be used in the application for funds from the RPCGB for future improvements.

Once Federal funds are in place for the project, an environmental document will need to be prepared. The environmental document must include technical studies and public involvement outreach necessary to comply with procedures of the National Environmental Policy Act (NEPA). Once the environmental study has been completed, the design would be undertaken, and construction would follow. Right-of-way acquisition and utility relocation would be conducted prior to construction.

Appendix A – Proposed Improvement Concepts

Appendix B – Study Area Right-of-Way Map

Appendix C – USFWS Threatened and Endangered Species

Appendix D – NRCS Web Soil Survey

Appendix E – EPA Environmental Justice Screening Tool Summary

Appendix F – Opinions of Probable Cost